

1.1 Welcome to the second public consultation for the design and development of a public realm and signage strategy for Cashel Town, in the heart of County Tipperary.

This stage forms part of an exciting and important process that aims to secure Cashel as a great place to **live in** and **work in**, as well as being a successful international tourist **destination**. As such, it forms part of the on-going partnership between Tipperary County Council and Fáilte Ireland, who are working alongside The Paul Hogarth Company in its development.



Map of Ireland, 1951
Córas Iompair Éireann

Cashel holds a **special place** in local, national and international hearts and minds; it aspires to become a world-class destination, and all development in Cashel will need to reflect these ambitions.

1.2 Process

Alongside the **site analysis** our design team asked the public about what sort of place Cashel can and should be: this has informed each of the Concept and Sketch Designs.

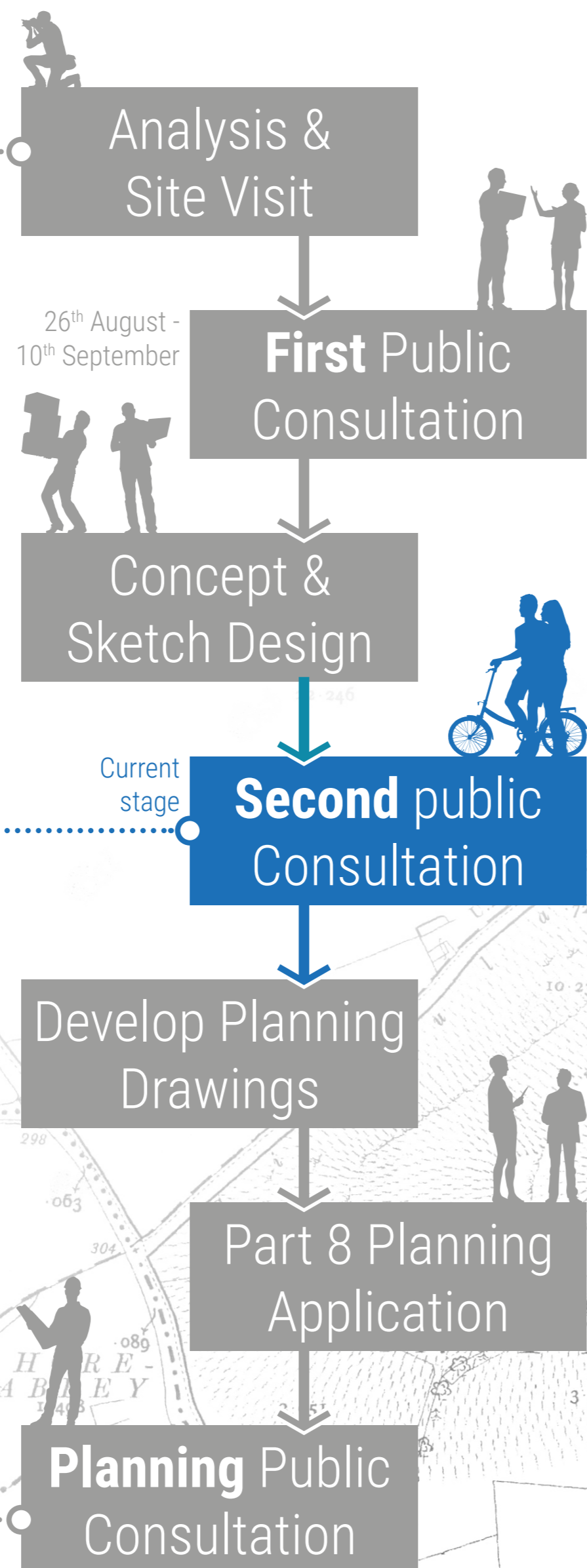
This second public consultation is an important part of the process. This is currently **where we are** in the process and, as such, the team is excited to hear your thoughts and feedback to this public consultation stage.

And then what will happen? Our multi-disciplinary team will work towards compiling these ideas into a package for **planning consultation**.

1.3 Have Your Say!

Take a look at these boards, let us know your thoughts and please fill in a questionnaire, or can also email the team at cashel@paulhogarth.com

Right Cashel Town 1897-1913
Ordnance Survey 25" Series

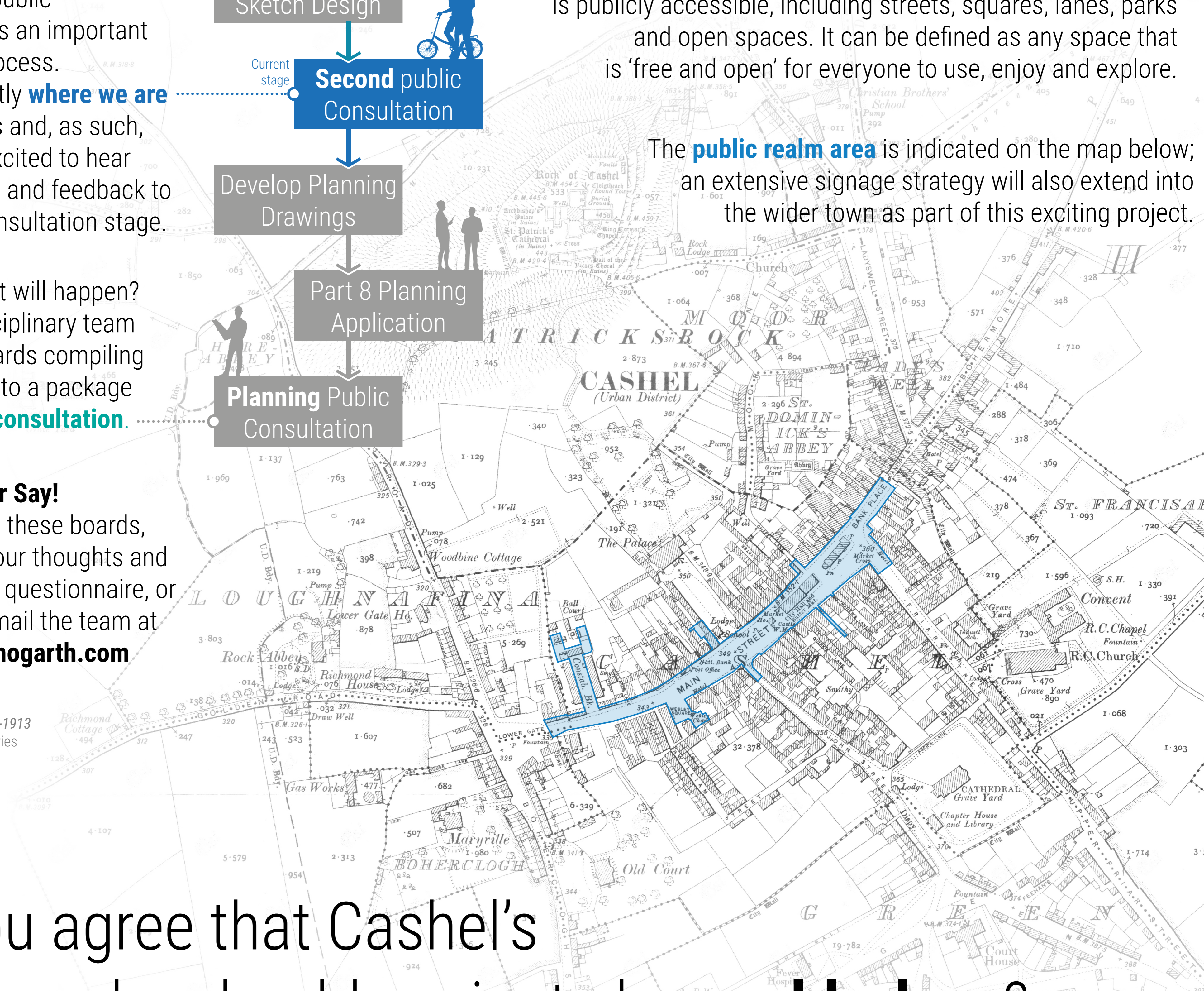


What is the public realm?

Public Realm is the space between and within buildings that is publicly accessible, including streets, squares, lanes, parks and open spaces. It can be defined as any space that is 'free and open' for everyone to use, enjoy and explore.

The **public realm area** is indicated on the map below; an extensive signage strategy will also extend into the wider town as part of this exciting project.

Do you agree that Cashel's public realm should aspire to be **world-class**?

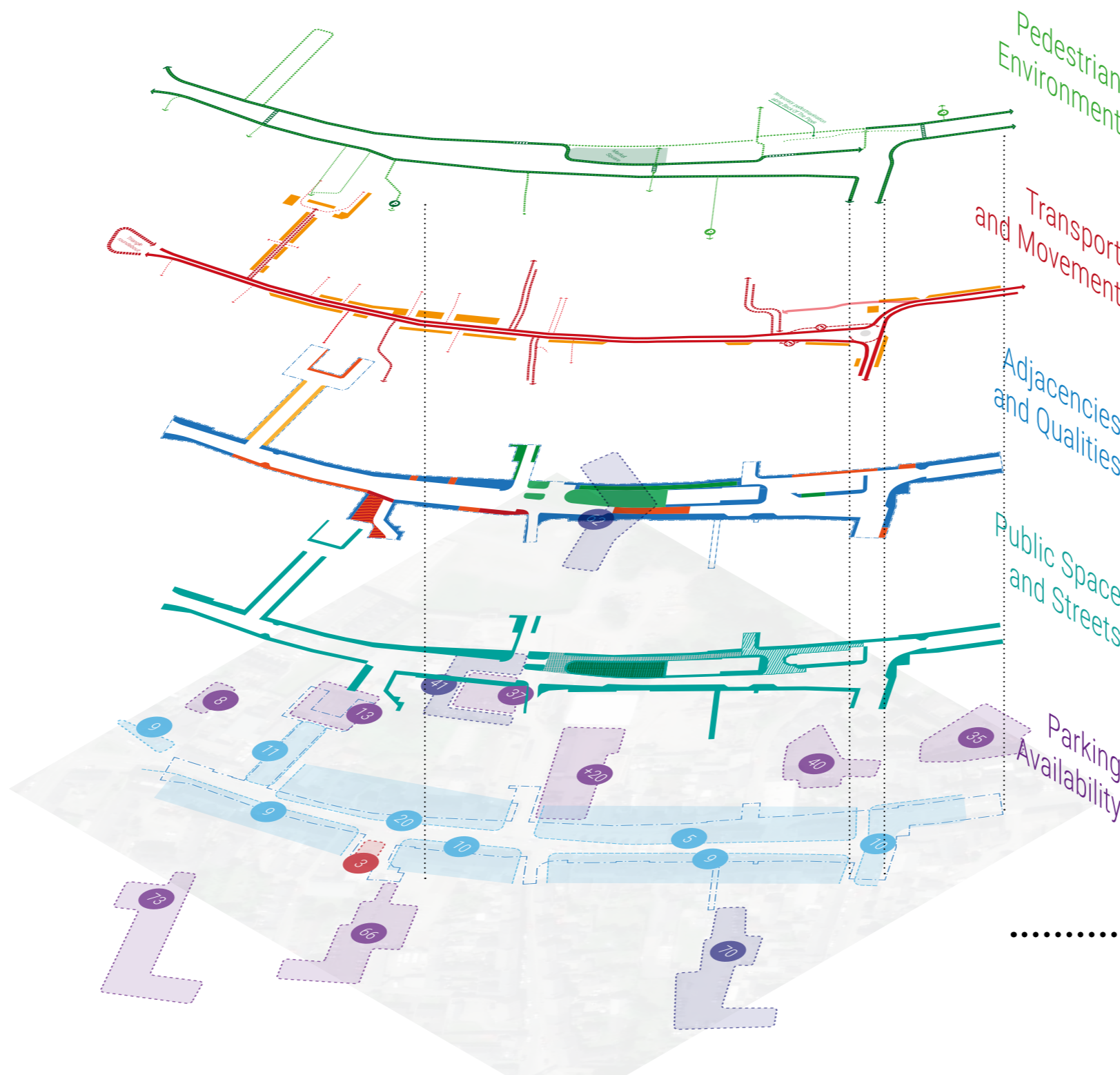


2.1 Findings

An extensive process of **public engagement** and **site and desktop analysis** has resulted in a comprehensive understanding of the town's rich history, its people, the requirements of its urban centre, and ideas that can further improve the town's special and unique sense of place.

2.2 Site and Desktop Analysis

Working alongside the town's local people and business stakeholders we have developed a detailed understanding of the physical, environmental, economic and social composition of Cashel.



A comprehensive network of pedestrian **footpaths** and **public spaces** weave through Cashel Town centre

The town remains a popular place for people to visit by **car**, but some traffic uses the Main Street just to pass through

While some areas are in **good condition**, this contrasts with other zones that are in need of an **upgrade**.

A diverse network of **spaces, laneways, paths** and **squares** are evident throughout the town centre.

A variety of on-street, off-street, public and private **parking** spaces are within a two-minute walk of the Main Street:

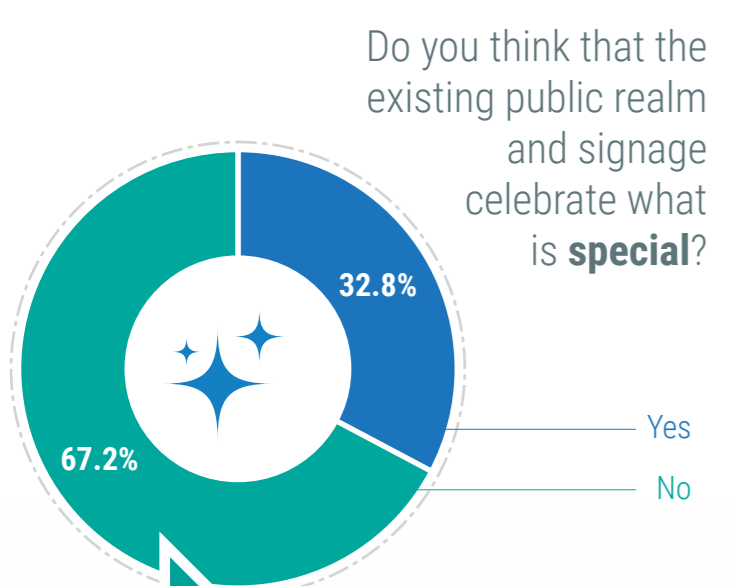
- 83 Public on-street
- 173 Public off-street
- 303 Private off-street

2.3 Consultation

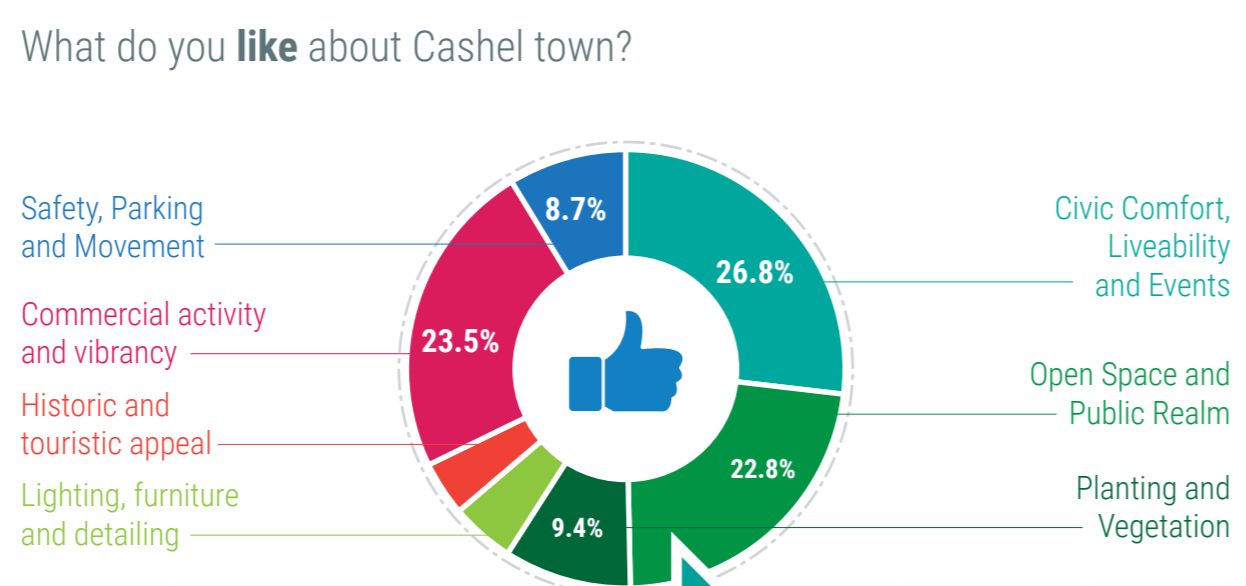
Through the first public consultation, the design team has enjoyed listening to the community and hearing local views on their town, to develop important **understandings** about how the town is currently perceived, used and visited, as well as finding out what it is that local people want.

What has become clear through the first public consultation is that there is a local appetite for quality public **events space** and an improved **public realm**

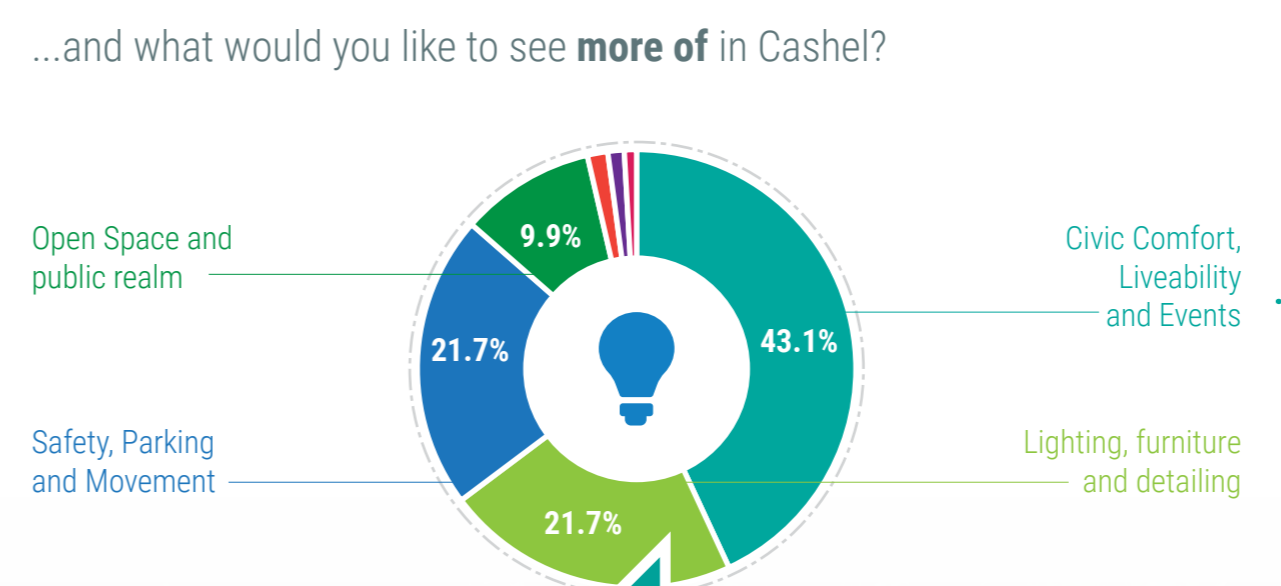
...and there is a particular interest in the provision of **markets** and **events** space in the town centre!



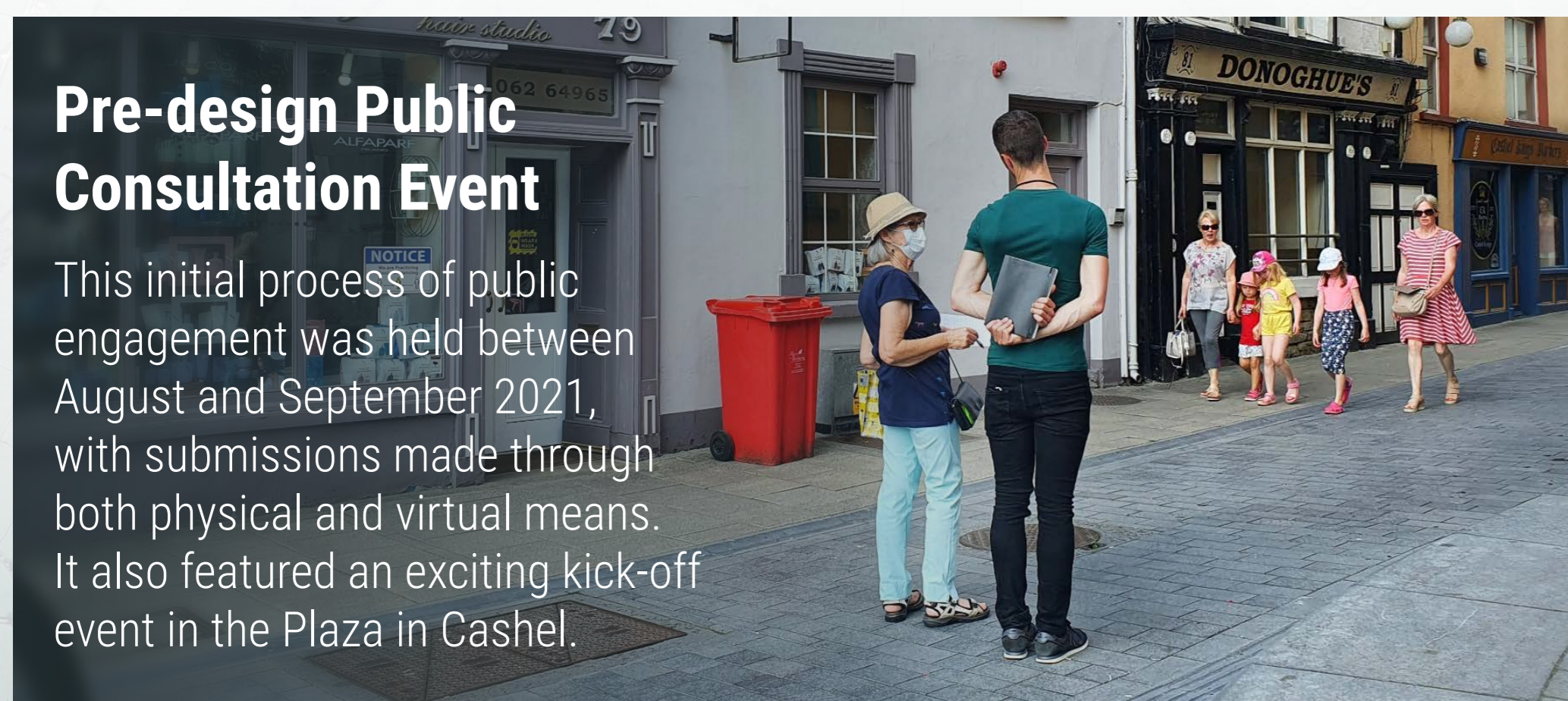
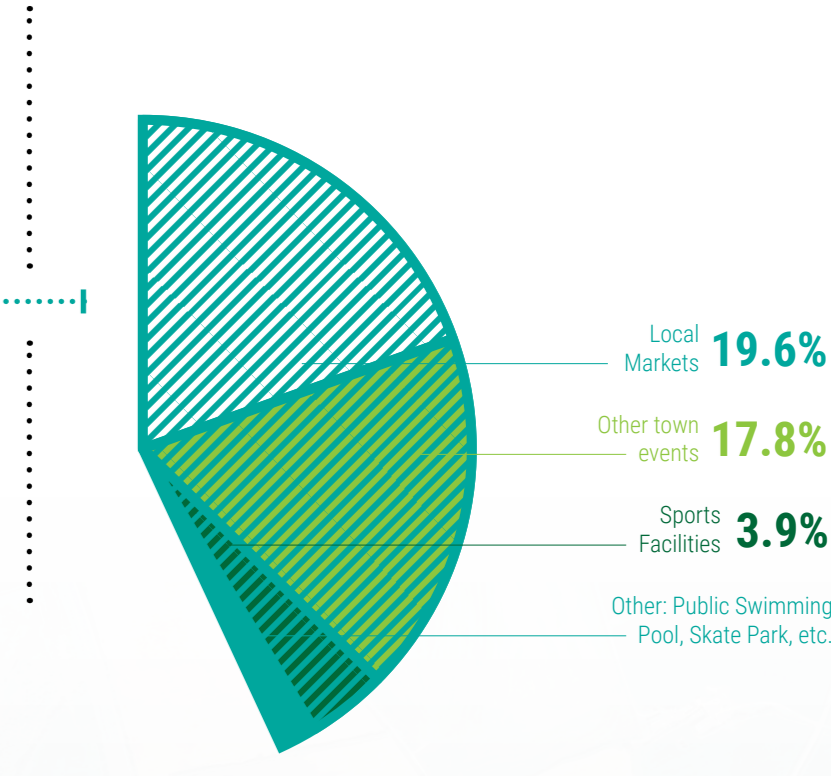
Most people think that the current public realm is under-performing



Local people value Cashel as a working and **vibrant town**



Local people want a better **balance** of vehicles and people spaces for walking and events



Pre-design Public Consultation Event

This initial process of public engagement was held between August and September 2021, with submissions made through both physical and virtual means. It also featured an exciting kick-off event in the Plaza in Cashel.

2.4 Written Responses

Respondents were asked for any of their **thoughts** and **ideas** about the town:

“ It would be really great to connect the town's important **spaces, places** and **historic assets**.

“ There is **a lot to celebrate** and point to, but so much of the existing tourism signage focuses on **The Rock** rather than **the town**.

“ Arriving into Cashel Town, there is no sense of its **centre**, and no clear idea of where is available to park.

Do you agree with the summary of **analysis** shown above?

3.1 Approach

With the enthusiastic local response presenting a diverse range of comments and ideas, we have used this to develop four **concept proposals** alongside a number of important principles that guide further design direction and development for the public realm strategy. These Concepts are:

A Town and Rock

To better **connect** the Town centre to The Rock, so as to **benefit** each of the town, the Rock and the wider region

B Main Street

To provide additional **safe** and **usable** open space on the Main Street for those who live in, work at and visit the town.

C Local Connections

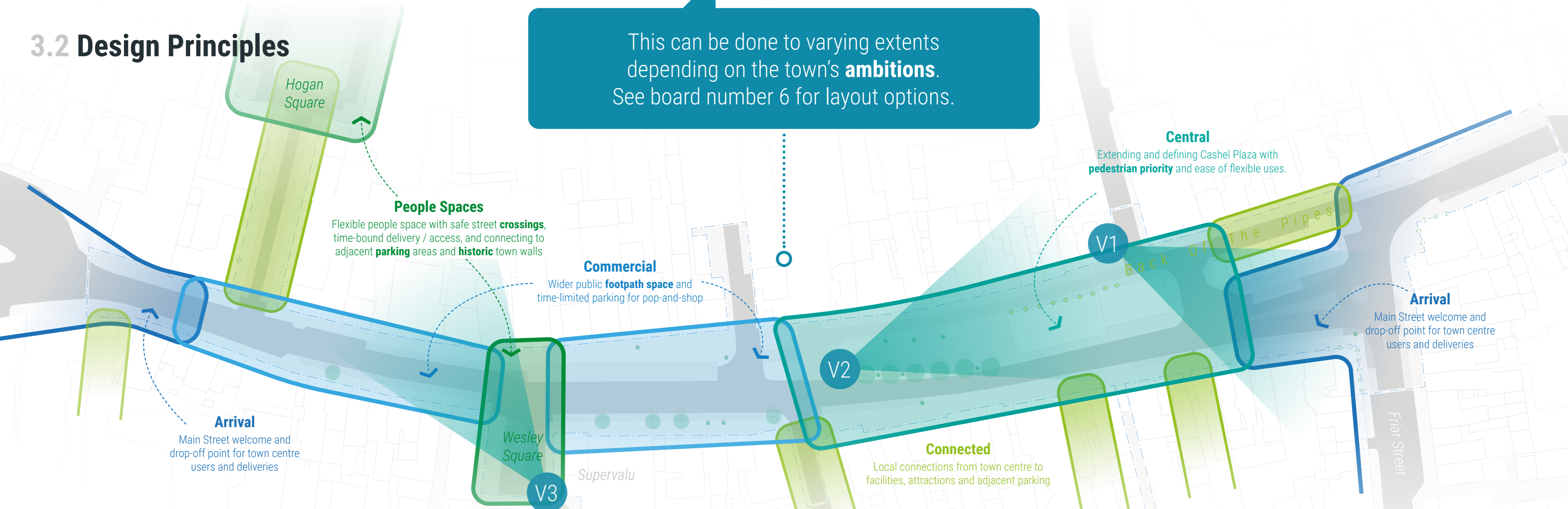
To develop Cashel Town's laneways as **unique** and **special** local **connections**, linking improved road crossings to the off-street carparks.

D Active Walks

To rationalise and **simplify** the town's and region's trail network, providing clear, unique and memorable visitor **experiences**.

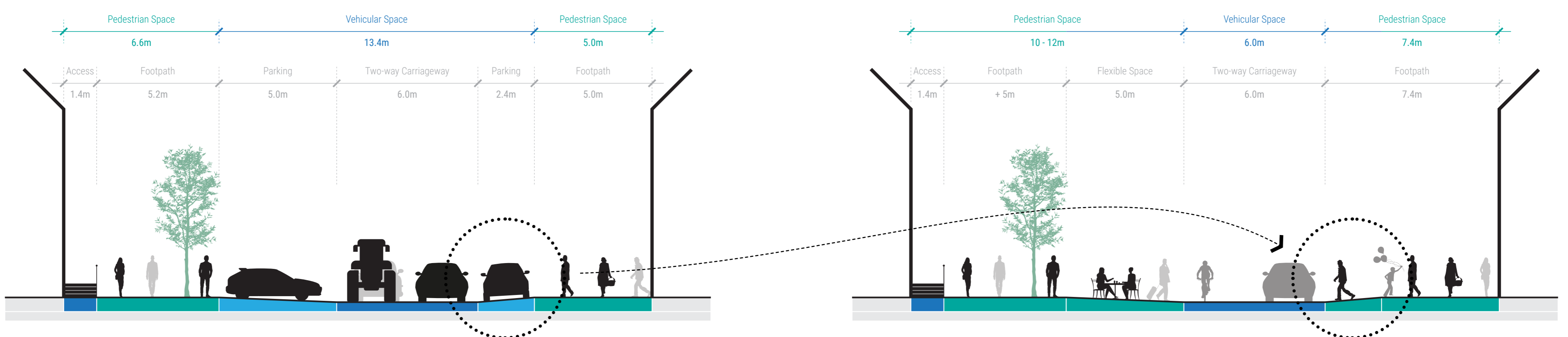
3.2 Design Principles

This can be done to varying extents depending on the town's **ambitions**. See board number 6 for layout options.



3.3 Flexible Street Space

Some on-street parking can provide a flexible dual function: allowing cars to park while at certain times, seasonally or in the future, providing additional people space for events, dining, seating and other functions of the public realm.



Do you think that these concepts would **benefit** Cashel?

4.1 The below visualisations show some of the key spaces along the Main Street, and what the proposed upgrades might look like.

Pictured below, the town's historic *Back Of The Pipes* is seen as a thriving pedestrian thoroughfare with space available for on-street dining and performance, as well as direct and safe connections between The Rock and the Plaza, and other homes and businesses along Main Street.

Currently, this area of the town provides almost all of its space for vehicles, leaving only tight areas for safe pedestrian movement (right)



Historic fabric of town to be rejuvenated and celebrated

Adaptable canopy providing shelter and shade



Retain elements of the existing space so as to reduce materials required and overall carbon impact

Palette of durable materials and surface finishes

Bespoke in-ground interpretation, signage and wayfinding elements

Wide and uncluttered outdoor space for seating, dining, public events and outdoor performance

Ensuring that the public space is uncluttered and durable creates the opportunity for dynamic and exciting uses such as events and performance



Do you think that this is an improved **heritage setting** for Back Of The Pipes?



V2 The Plaza

A space that is next to a loud and busy thoroughfare, the plaza currently struggles to retain vibrant urban life throughout the day.

Do you think that a **canopy** would be a positive addition to the Plaza?

Overhead canopy offering shelter and sun shade

Existing mature trees repaired and retained

Flush carriageway with reduced speeds

Existing steps function as seating and local performance space



V3 Wesley Square

Existing conditions push pedestrians to the **edge** of the space, while providing unsafe **street crossings** to access local shops, amenities and town centre attractions.

Legible on-street wayfinding system

Removeable overhead canopy system

Extended footpath and people space

Raised table crossings

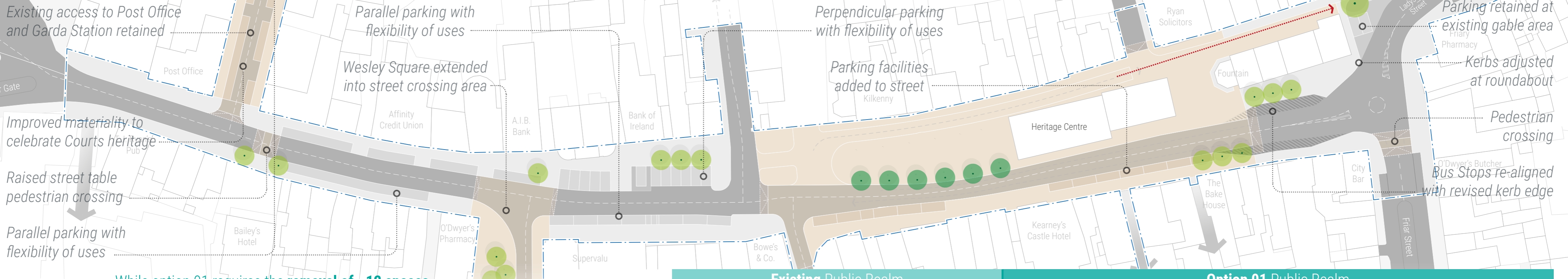
Does this idea balance the need for access with **safe pedestrian space**?



Expanded Footpaths

Option 01

Option 01 responds to the aspiration of retaining the **existing street network** and the majority of **on-street parking** along Main Street. As such, some additional space for seating, lighting, planting and safe pedestrian movement is provided simply through the adjustment of existing kerb lines.



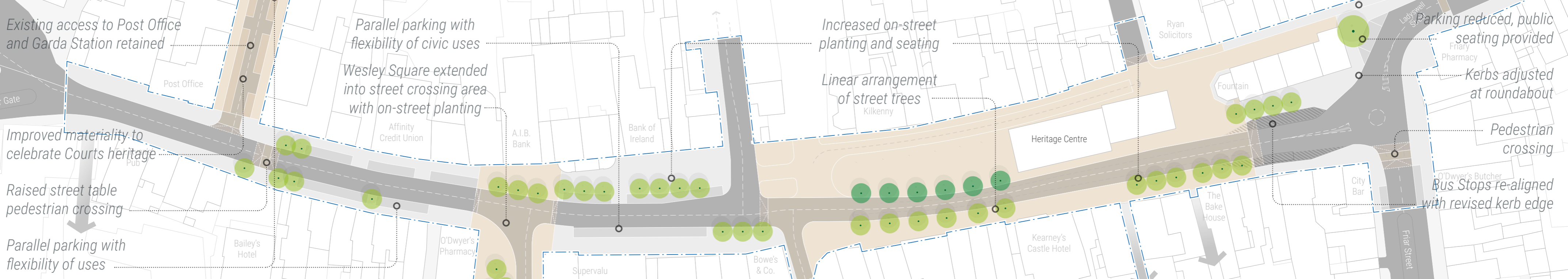
While option 01 requires the **removal of ~12 spaces** (~7 of which are due to essential design compliance), the proposal to reduce the maximum stay time at on-street spaces to 1 hour can **increase the parking capacity** of the street by 196 opportunities to park, each day. Those who want to park on Main Street for longer can be encouraged to use new and improved off-street facilities.

On-street Car parking Summary: Option 01	Existing Public Realm			Option 01 Public Realm			Capacity when reduced to 1-hour maximum stay	
	Quantity	Capacity based on current 2-hour maximum stay time	Capacity (2 hr. stay)	Vehicle Parking Provided	Removed due to Compliance	Design	Duration	Capacity
Total	73	2 hr. max.	292	61	7	5	1 hr. max.	488
Change				-12				+ 196 no.

Increased Public Space

Option 02

Option 02 acknowledges the desire for increased pedestrian space and a generous public realm. The current **vehicle network is retained** along Main Street, however a **reduction in the on-street parking** spaces allows for an increase in safer space for pedestrians, cyclists and other street users and visitors.



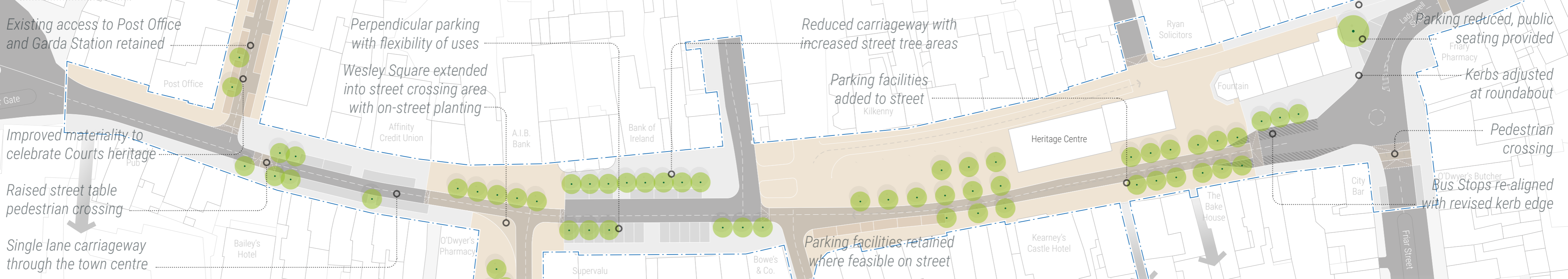
While option 02 requires the **removal of ~39 spaces** (~7 of which are due to essential design compliance), the proposal to reduce the maximum stay time at on-street spaces to 1 hour would **reduce the parking capacity** of the street by just 20 opportunities to park, each day. Those who want to park on Main Street for longer can be encouraged to use new and improved off-street facilities.

On-street Car parking Summary: Option 02	Existing Public Realm			Option 02 Public Realm			Capacity when reduced to 1-hour maximum stay	
	Quantity	Capacity based on current 2-hour maximum stay time	Capacity (2 hr. stay)	Vehicle Parking Provided	Removed due to Compliance	Design	Duration	Capacity
Total	73	2 hr. max.	292	34	7	32	1 hr. max.	272
Change				-39				- 20 no.

Pedestrian Priority Centre

Option 03

Option 03 recognises the desire for **generous on-street public space** and **ample car parking** along Main Street. This scheme reduces the existing two-way network to a **one-way system** - reducing town centre traffic - while offering more space for street improvements, and ensuring that local service deliveries remain possible.



While option 03 requires the **removal of ~29 spaces** (~5 of which are due to essential design compliance), the proposal to reduce the maximum stay time at on-street spaces to 1 hour can **increase the parking capacity** of the street by 60 opportunities to park, each day. Those who want to park on Main Street for longer can be encouraged to use new and improved off-street facilities.

On-street Car parking Summary: Option 03	Existing Public Realm			Option 03 Public Realm			Capacity when reduced to 1-hour maximum stay	
	Quantity	Capacity based on current 2-hour maximum stay time	Capacity (2 hr. stay)	Vehicle Parking Provided	Removed due to Compliance	Design	Duration	Capacity
Total	73	2 hr. max.	292	44	5	24	1 hr. max.	352
Change				-29				+ 60 no.

Which option do you think is the most **appropriate** for an **ambitious heritage town** like Cashel?